



We connect Bavaria

Roads, bridges and cycleways



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Cover image:
B 289, Schorgasttal bridge as part of
the Untersteinach bypass

Mobility is the cornerstone of our society and crucial to the economic strength and prosperity of our state. Particularly in a state as large as Bavaria, where our citizens and businesses depend on a well-developed and safe road network. With an effective and efficient infrastructure, we promote equal living conditions in both town and country!

The Free State of Bavaria is committed to a transport policy that makes optimum use of the strengths of all modes of transport and combines them. The aim is to create a modern, affordable and sustainable form of mobility that offers a wide range of options. While cars remain indispensable, especially in rural areas, public transport and cycling are becoming increasingly important in urban areas. A well-developed road network is also essential for making public transport attractive.

We are committed to roads as a means of transport. Our Road and Bridge Building Division is responsible for more than 20,000 kilometres of federal and state roads and more than 6,500 kilometres of cycleways. We also manage over 3,000 kilometres of district roads. By building, upgrading and widening many new roads and focusing on maintenance, we are making lasting improvements to the condition and safety of our roads.

Many of Bavaria's bridges are masterpieces of civil engineering, combining high architectural standards with the task of connecting our home state. Their safe and stable construction promotes traffic efficiency and underlines our efforts to develop infrastructure in a forward-looking and sustainable manner.



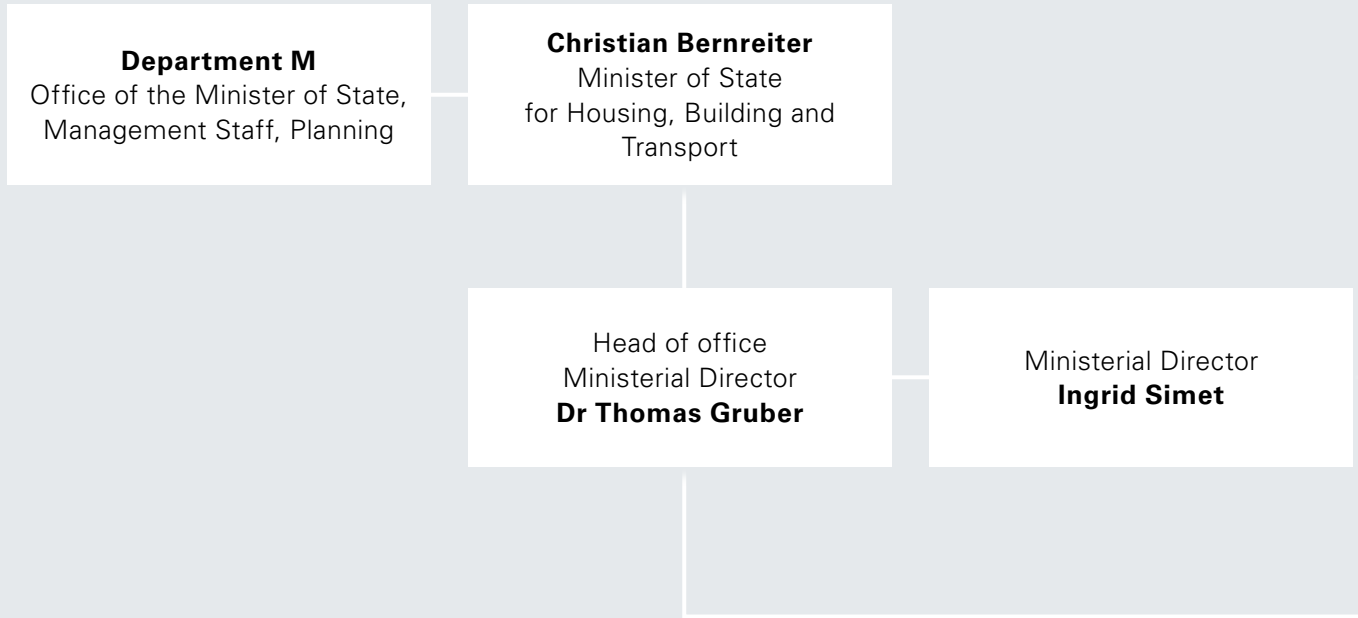
As Minister of Transport, cycling is a particularly important issue for me. The promotion of cycling is a central component of Bavaria's mobility and transport policy. Our aim is to encourage more and more people to use bicycles for everyday and leisure travel, because cycling is space-saving, healthy and promotes sustainable mobility.

The Free State Of Bavaria is pushing ahead with the development of cycleway infrastructure: the new Bavarian Cycling Act has been in force since August 2023. Together with our local authorities, we want to create a total of 1,500 kilometres of new cycleways in Bavaria by 2030. The Free State itself is building cycleways on federal and state roads, and is helping local authorities to build and develop their own lanes, because only together can we move cycling forward!

This brochure provides you a brief overview of the services provided in Bavaria by the state building authorities for road and bridge building. Thank you for your interest and have a safe and pleasant journey on Bavaria's roads!

A handwritten signature in blue ink, appearing to read 'Christ. Bernreiter', written in a cursive style.

Christian Bernreiter
Bavarian State Minister for
Housing, Building and Transport



ORGANISATION

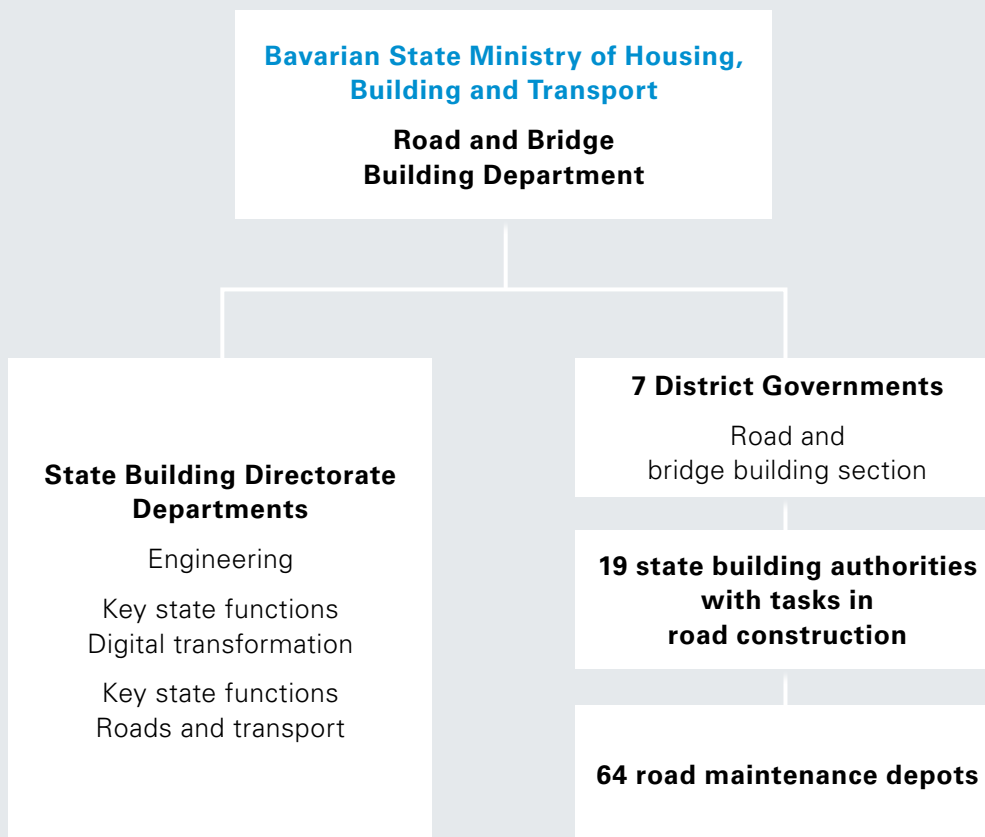
Responsibilities and structure

In addition to building and urban development, housing and transport, road and bridge building is one of the central areas of responsibility of the Bavarian State Ministry of Housing, Building and Transport. The Road and Bridge Building Department is divided into ten divisions and includes responsibility for federal and state roads, road maintenance and civil engineering, which includes bridge and tunnel building. In addition to our responsibilities in landscape planning and road safety, we also work in collaboration with "Die Autobahn GmbH des Bundes", the company that manages the federal motorways.



Roads and Bridges

The Bavarian State Ministry of Housing, Building and Transport is the supreme state authority for road and bridge building in Bavaria. The seven administrative districts fulfil centralised tasks on behalf of the intermediate authorities. Their remit also encompasses the processing of planning approvals and funding for local road building projects. The Bavarian State Building Directorate is another intermediate authority responsible for centralised state functions in road building and for the twelve central road construction offices. Our 19 state building authorities, which are responsible for road building, form the basic level of the Bavarian State Building Administration. They are tasked with the administration, planning, building and operation of federal and state roads, as well as some district roads. The 64 road maintenance depots that belong to the state building authorities ensure that the roads are kept in a good state of repair.



Areas under the responsibility of the state building authorities



- State Ministry of Housing, Building and Transport
- State building authorities with tasks in road construction
- State building authority service centres
- ◆ State Building Directorate
- ▲ District Governments

FIGURES AND DATA

Route network

With our 19 state building authorities, we are responsible for over 20,000 kilometres of federal and state roads and over 6,500 kilometres of cycleways. There are also around 11,600 bridges and 46 road tunnels in this road network.

The administrative districts have the option of having us manage their district road network, which means we also look after more than 3,000 kilometres of district roads and carry out design and building work for the districts.

In addition to local authorities and elected officials, we work closely with local building and engineering companies. Our total building investment in 2023 was almost EUR 1 billion. In addition, the Free State of Bavaria injects more than EUR 200 million per annum for the expansion and building of local roads.

B 318, grade-separated upgrade of the junction with the MB 19 district road in Warngau

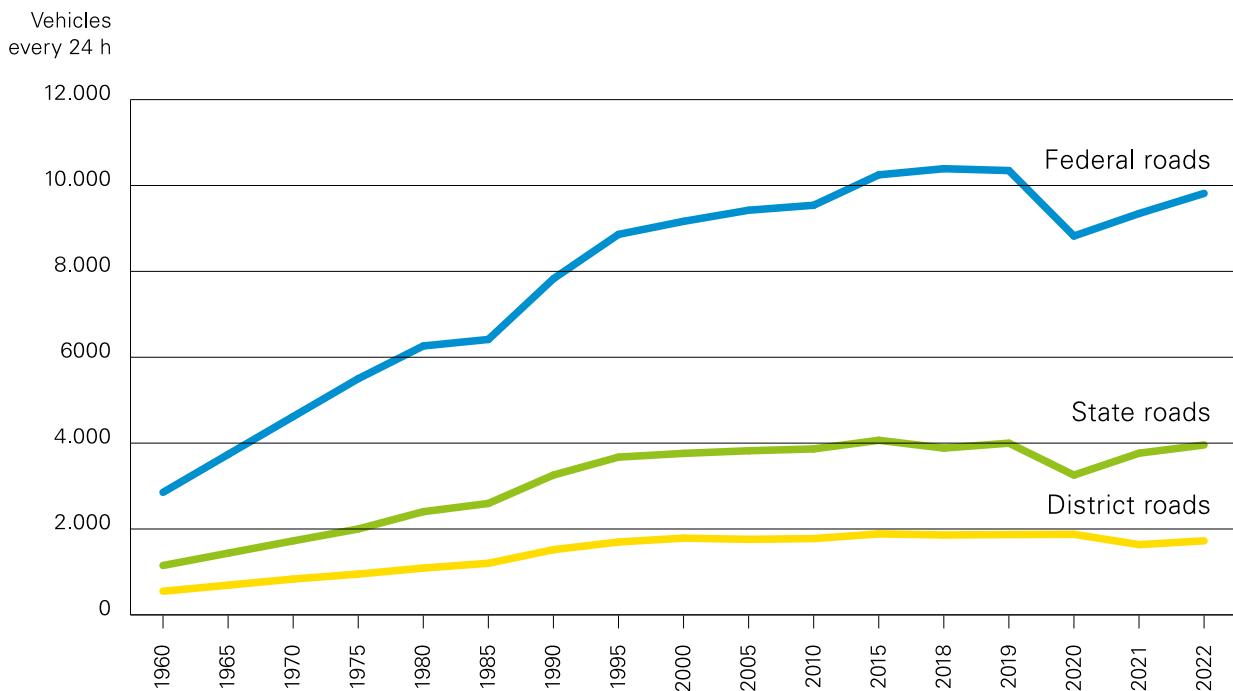


Increasing traffic congestion in many parts of Bavaria makes it necessary to maintain and continue to improve the condition of the existing network.

As a result of a reform of the motorway administrations of the federal states initiated by the federal government, the previous management of the federal motorways was transferred to the federal government's newly established company "Die Autobahn GmbH des Bundes" on 1 January 2021.

In Northern Bavaria, the Northern Bavaria branch in Nuremberg and in Southern Bavaria the Southern Bavaria branch in Munich are now responsible for all tasks on the federal motorways.

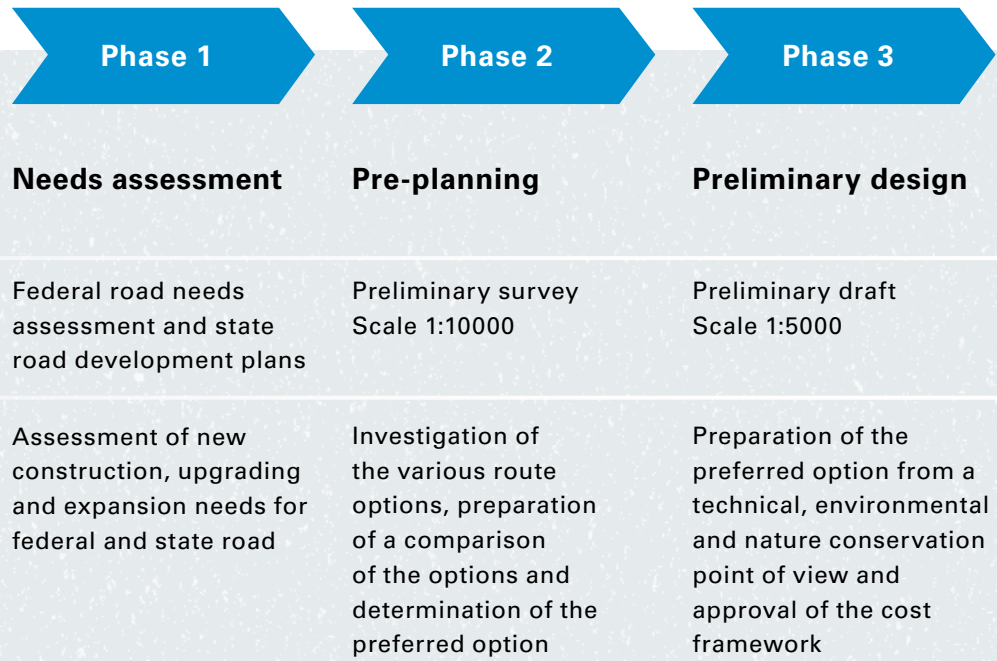
Average daily traffic development



PLANNING

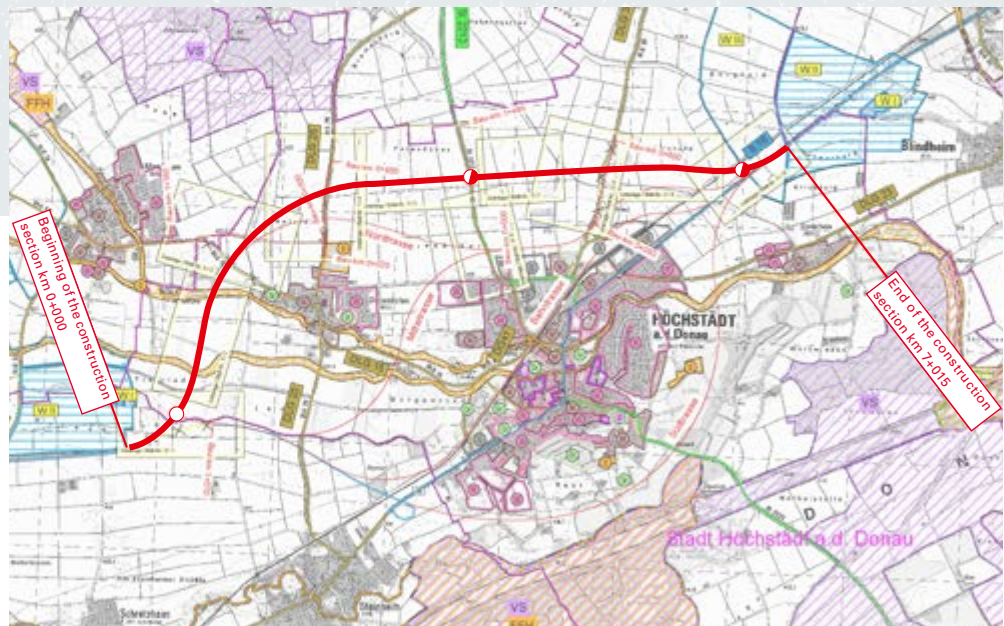
The road planning process

The planning of major new road construction, upgrading and expansion projects on federal and state roads is essentially divided into five phases, from the determination of requirements through to contract-ready design.



Further information on the Federal Transport Infrastructure Plan and the Expansion Plan is available here: www.baysis.bayern.de (in German only)

Example of a new building project included in the Federal Transport Infrastructure Plan. The overview plan shows the protected areas to be considered, existing land uses, and the alternatives investigated.



Phase 4

Development consent planning (building regulations)

Approval design
Scale 1:1000

A planning approval process, in which all issues pertaining to public law are discussed with the public and the authorities, is usually required to obtain planning permission.

Phase 5

Execution planning

Construction design
Scale 1:500

Preparation of detailed building plans for the contractors

CONSTRUCTION PHASE

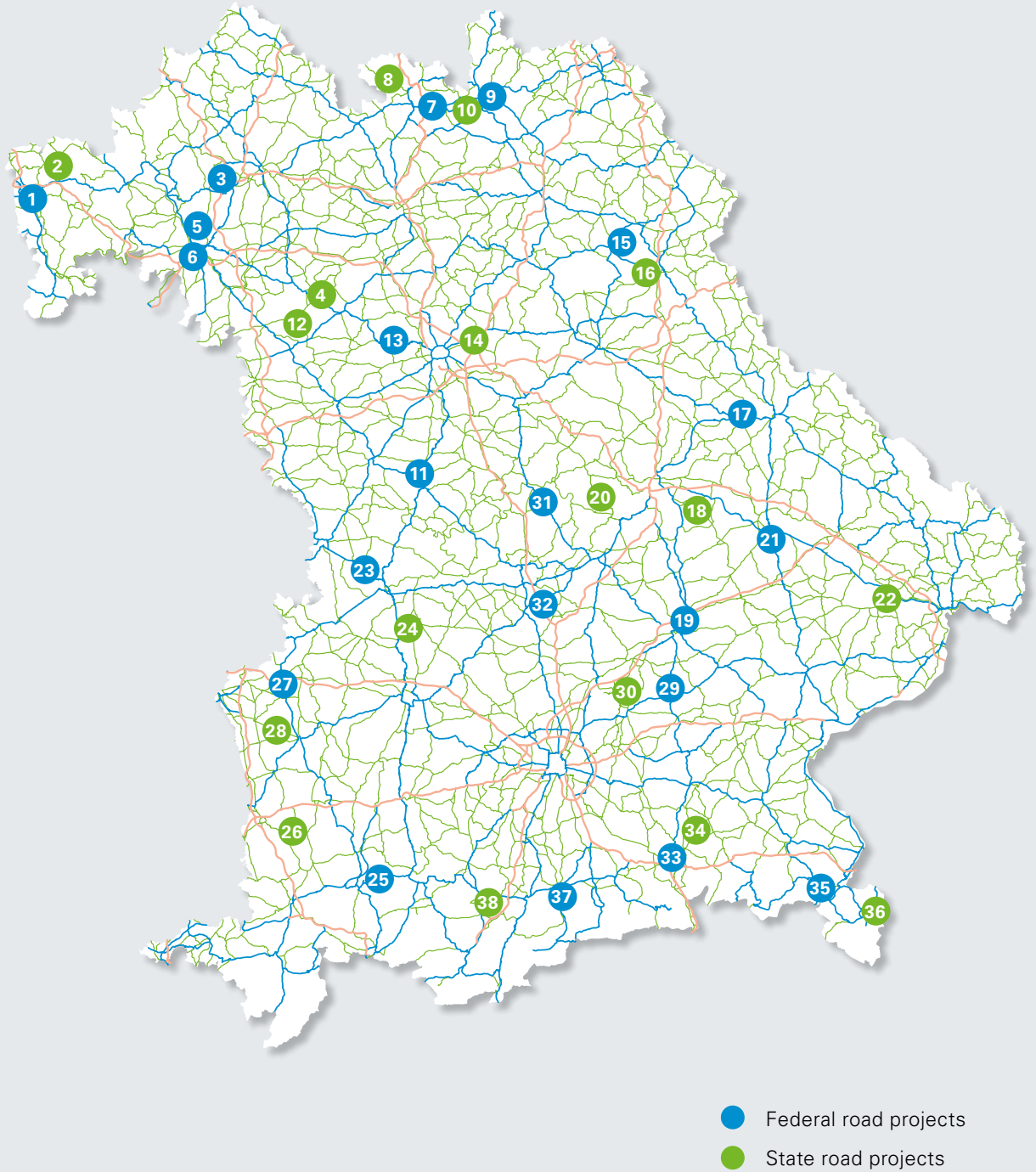


Example of an overview plan created as part of the execution planning process. The aerial photograph shows, among other things, the traffic areas, the embankments, the buildings and the rain water retention basins.

CONSTRUCTION

Ongoing building measures

The map show selected projects from our 19 state building authorities on federal and state roads. The measures provide an overview of the portfolio of different building projects by our state building authorities.





1 B26 Extension to Darmstädter Straße - access to the port

Aschaffenburg state building authority
 Length: 1.4 km
 Total costs: EUR 15 m
 Completion in 2024
 Photo: Hajo Dietz, Nürnberg Luftbild



2 St2307 Road resurfacing between Schimborn and Hösbach

Aschaffenburg state building authority
 Length: 5.3 km
 Total costs: EUR 1.6 m
 Completion in 2023



3 B286 Hahnenhügel bridge, replacement of the end cross beam

Schweinfurt state building authority
 Length: 0.3 km
 Total costs: EUR 0.8 m
 Completion in 2023



4 St2286 Road widening between Frickenhausen and Hainhof

Schweinfurt state building authority
 Length: 2.9 km
 Total costs: EUR 9.7 m
 Completion in 2024



5 B26 Modernisation of the viaduct near Binsfeld

Würzburg state building authority
 Length: 0.1 km
 Total costs: EUR 6.3 m
 Completion in 2023
 Photo: Jan R. Schäfer, J1-Fotografie und Luftbildservice



6 B27 Road resurfacing between Thüngersheim and Veitshöchheim

Würzburg state building authority
 Length: 4.8 km
 Total costs: EUR 4.9 m
 Completion in 2023
 Photo: Jan R. Schäfer, J1-Fotografie und Luftbildservice

UPPER FRANCONIA



7 B173 Extension between Lichtenfels and Zettlitz, 3rd building phase

Bamberg state building authority

Length: 8.0 km

Total costs: EUR 143.5 m

Completion in 2026

Photo: Hajo Dietz, Nürnberg Luftbild



8 St2205 Relocation north of Coburg

Bamberg state building authority

Length: 6.2 km

Total costs: EUR 46.4 m

Completion in 2025



9 B289 Kauerndorf bypass

Bayreuth state building authority

Length: 1.9 km

Total costs: EUR 90.1 m

Completion in 2032



10 St2190 Slope stabilisation south of Kasendorf

Bayreuth state building authority

Length: 1.5 km

Total costs: EUR 0.9 m

Completion in 2023

MIDDLE FRANCONIA



11 B 13/B2 Resurfacing of Eichstätt junction

Ansbach state building authority
Length: 0.6 km
Total costs: EUR 0.4 m
Completion in 2023



12 St2253 Upgrading north of Berolzheim

Ansbach state building authority
Length: 1.3 km
Total costs: EUR 3.9 m
Completion in 2023



13 B 8 Modernisation of Zenntal bridge near Langenzenn

Nuremberg state building authority
Length: 0.5 km
Total costs: EUR 4.5 m
Completion in 2023
Photo: Hajo Dietz, Nürnberg Luftbild



14 St2225 Modernisation of Unterrödel through road

Nuremberg state building authority
Length: 1.1 km
Total costs: EUR 3.3 m
Completion in 2023
Photo: Hajo Dietz, Nürnberg Luftbild

UPPER PALATINATE



15 B299 Upgrade north-east of Hessenreuth

Amberg-Sulzbach state building authority
Length: 4.7 km
Total costs: EUR 25 m
Completion in 2023
Photo: Drone images: Weiden



16 St2166 Modernisation of the bridge over the River Haidenaab near Mantel

Amberg-Sulzbach state building authority
Length: 100 m
Total costs: EUR 8.0 m
Completion in 2023
Photo: Oliver Heidl



17 B85 4-lane expansion east of Altenkreith - west of Wetterfeld

Regensburg state building authority
Length: 3.4 km
Total costs: EUR 51.9 m
Completion in 2023
Photo: Josef Kerscher



18 St2111 Modernisation of the railway bridge at Aukofen with the construction of a groundwater storage basin

Regensburg state building authority
Length: 200 m
Total costs: EUR 5.1 m
Completion in 2023

LOWER BAVARIA



19 B 15 Noise barrier near Ergolding

Landshut state building authority
Length: 740 m
Total costs: EUR 2.95 m
Completion in 2023



20 St2233 Widening north of Painten

Landshut state building authority
Length: 1.4 km
Total costs: EUR 2.2 m
Completion in 2022



21 B 8 Cycleway between Straubing and Aiterhofen

Passau state building authority
Length: 2.0 km
Total costs: EUR 0.7 m
Completion in 2023



22 St2083 Vilshofen bypass

Passau state building authority

Length: 8.6 km

Total costs: EUR 43.1 m

Under construction until 2024

LOWER BAVARIA





23 B 25 Modernisation of Wörnitzstein railroad overpass with line extension

Augsburg state building authority
Length: 0.6 km
Total costs: EUR 7.6 m
Under construction until 2024



24 St2045 Pilot noise barrier project with photovoltaics south of Waltershofen

Augsburg state building authority
Surface area: 1046 m²
Total costs: EUR 1.1 m
Completion in 2023



25 B 472 Upgrade east of Marktoberdorf, construction phases 2-4

Kempten state building authority
Length: 5.1 km
Total costs: EUR 26.2 m
Under construction until 2024



26 St2011 Conversion of Huber junction in Ottobeuren

Kempten state building authority
Length: 0.3 km
Total cost: EUR 1.8 m
Completion in 2023



27 B 16 Munasenke extension north of Kleinkötz, construction phase 2

Krumbach state building authority
Overall length: 0.7 km
Total cost: EUR 4.6 m
Under construction until 2024



28 St2019 Widening between Ingstetten and Deisenhausen

Krumbach state building authority
Overall length: 5.8 km
Total cost: EUR 12.1 m
Under construction until 2024

UPPER BAVARIA



29 B388 Taufkirchen/Vils bypass

Freising state building authority
Overall length: 5.4 km
Total cost: EUR 52.2 m
Under construction until 2027



30 St2082 Widening of the Wartenberg through road

Freising state building authority
Overall length: 0.8 km
Total cost: EUR 2.2 m
Completion in 2023



31 B299 Pilot BIM roundabout project in Beilngries

Ingolstadt state building authority
Length: 0.3 km
Total cost: EUR 1.7 m
Completion in 2023



32 B300/St2049 Remodelling of Langenbruck junction

Ingolstadt state building authority
Length: 0.3 km
Total cost: EUR 1.1 m
Completion in 2023
Photo: Hajo Dietz, Nürnberg Luftbild



33 B 15 Westtangente Rosenheim - construction phases 2 and 3.1

Rosenheim state building authority

Length: 4.5 km

Total cost: EUR 148 m

Completion in 2023

Photo: Aerial photo: Alfred Schellmoser



34 St2095 Widening of the Bamham through road and modernisation as far as Waldering

Rosenheim state building authority

Length: 2.6 km

Total cost: EUR 3.1 m

Completion in 2023



37 B 13 Cycleway north of Lenggries

Weilheim state building authority

Length: 6.0 km

Total cost: EUR 2.9 m

Completion in 2023

UPPER BAVARIA



35 B305 Modernisation Samer Bridge near Schneizreuth

Traunstein state building authority
Length: 100 m
Total cost: EUR 2.5 m
Under construction until spring 2024



36 St2100 Upgrade between Bischofswiesen and Aschauerweiher

Traunstein state building authority
Length: 1.2 km
Total costs: EUR 4.4 m
Under construction until spring 2024



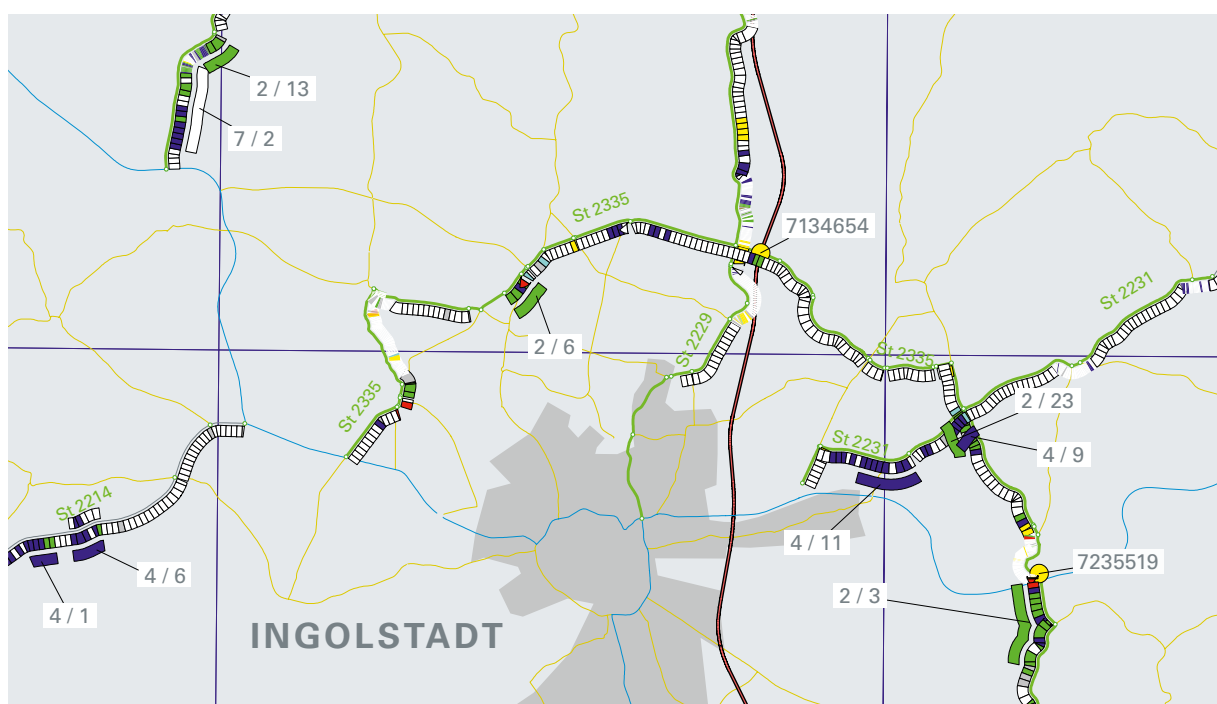
38 St2062 Flood release in Murnau

Weilheim state building authority
Length: 0.8 km
Total cost: EUR 8.9 m
Completion in 2023

MAINTENANCE

Maintenance management

The condition of roads at federal and state level is the subject of surveys every four years. This survey was repeated on the federal roads in 2023. It will be repeated on state roads in 2024. Regular inspections are also carried out on engineering structures such as bridges and tunnels. Maintenance needs are assessed from this and a maintenance strategy is developed. Our state building authorities draw up maintenance programmes and carry out the necessary work.



Map showing automatically generated maintenance sections and structures in need of refurbishment on state roads, used as the basis for developing maintenance programmes.



Multifunctional measurement system for assessing the surface condition of carriageways



B 13, road resurfacing near Eitensheim

Maintenance measures

Alongside new construction, upgrading and expansion measures, maintenance of the existing network is a key focus for our state building authorities. This includes not only maintenance and repairs but also complete roadway and structure renewals.



St 2275, Repair of the flood bridge on the River Main at Haßfurt

STRUCTURAL ENGINEERING

Civil engineering works

Civil engineering works are an important part of the roads we use. They bridge valleys and rivers, provide connections between intersecting roads, and protect people from noise and natural hazards. As an expression of building culture, they play an important role in shaping the urban landscape.

Regular structural inspections ensure the safety of bridges, tunnels, noise barriers and retaining walls on Bavaria's network of federal, state and district roads.





St2145, Modernisation of the bridge over the River Regen at Nittenau

Bridges

A large proportion of the 11,600 bridges in Bavaria's network of federal, state and district roads were built between 1960 and 2000. The oldest of these structures are often no longer able to meet today's load-bearing requirements. When these structures were designed and built, there was no way of predicting the increase in heavy traffic that has occurred in recent decades. Simple repairs are no longer enough in this case. They need to be strengthened or modernised.



B 289, Visualisation of the western portal of the Kauerndorf bypass tunnel

Tunnel

Tunnels were almost exclusively built for topographical reasons in the past. Today, tunnel solutions are increasingly demanded for road projects for environmental reasons.

Since tunnel building is complex and expensive, the advantages and disadvantages of a tunnel solution over an open cut route need to be identified and weighed up during the planning process.

We currently have 46 road tunnels in operation, with a total length of around 22 kilometres.



B 2, Rockfall protection fence between Eschenlohe and Oberau



Geohazards

Rockfalls, landslides, avalanches, mudflows and sinkholes can threaten the safety of road users and infrastructure. Climate change is intensifying these processes in some regions. To enable early countermeasures to be taken, potentially affected sections of roads are regularly inspected.

To ensure the safety of the road network, there are currently around 2,000 special safety structures and around 12,000 individual installations against avalanches and other hazards that need to be maintained and regularly inspected.



St2392, Rock clearance under full closure of the road at Kipfenberg in the Altmühltal valley



CYCLE PATHS

Cycling infrastructure

The promotion of cycling is a key focus of Bavarian transport policy. In order to further increase the share of cycling, well-developed and safe cycleways are particularly important.

That's why one of the main goals of the Bavarian Cycling Act, which came into force on 1 August 2023, is to build a total of 1,500 kilometres of new cycleways by the end of 2030 (compared to the end of 2022), in cooperation with the local authorities.

The Free State of Bavaria and its state building authorities are responsible for cycleways at federal roads for which the Federal Government is responsible and for cycleways at state roads for which the Free State of Bavaria is responsible. In 2023, new cycleways were built along more than 110 kilometres of federal and state roads.

The Free State of Bavaria offers attractive funding programmes to help local authorities build cycleways. In order to further encourage construction and accelerate the expansion of the cycleway

network, it has broadened both the funding criteria and further increased the funding rates. Under the Bavarian Municipal Transport Financing Act and the Bavarian Fiscal Equalisation Act, it provides the local authorities with a total of EUR 260 million per year for the construction of roads and cycleways. There are also funding programmes specifically for investment in cycling. One example is the cycling campaign "Climate State Bavaria". In 2023, EUR 10 million were made available to support local authorities.



St2409, Footpath and cycleway near Ammerndorf

Investment in roads remains at record level.






FUNDING

Budget

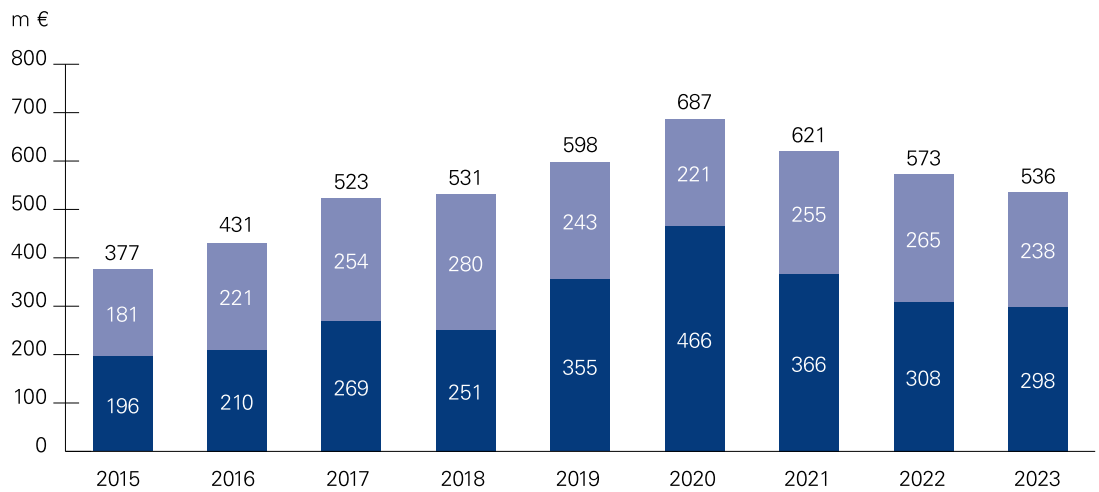
For many years, the annual investment in the new construction, upgrading and expansion of the federal roads has been in excess of EUR 500 million. In 2023, EUR 536 million were invested.

Investment in state roads will continue at record level. After exceeding EUR 400 million for the first time in 2022, a new record high of EUR 441 million was reached in 2023. Of this, almost two thirds, or EUR 285 million was invested in maintenance.

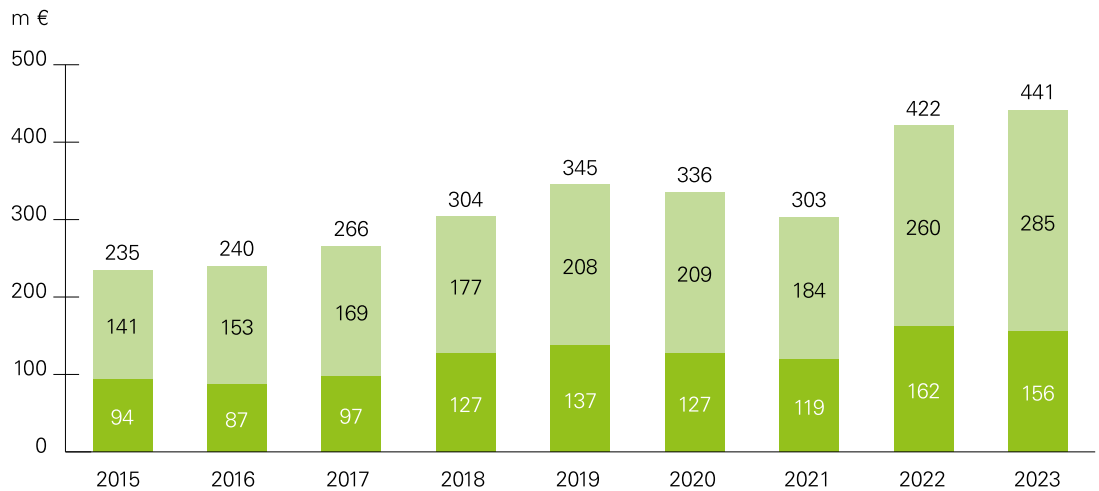
In addition, the Free State supports local authorities with fixed sum and project-based funding for their road and cycleway construction measures. In 2023, local authorities benefited from EUR 238 million of state funding for projects.

Maintenance  
New construction, upgrading and expansion  
Funding for local authority road projects 

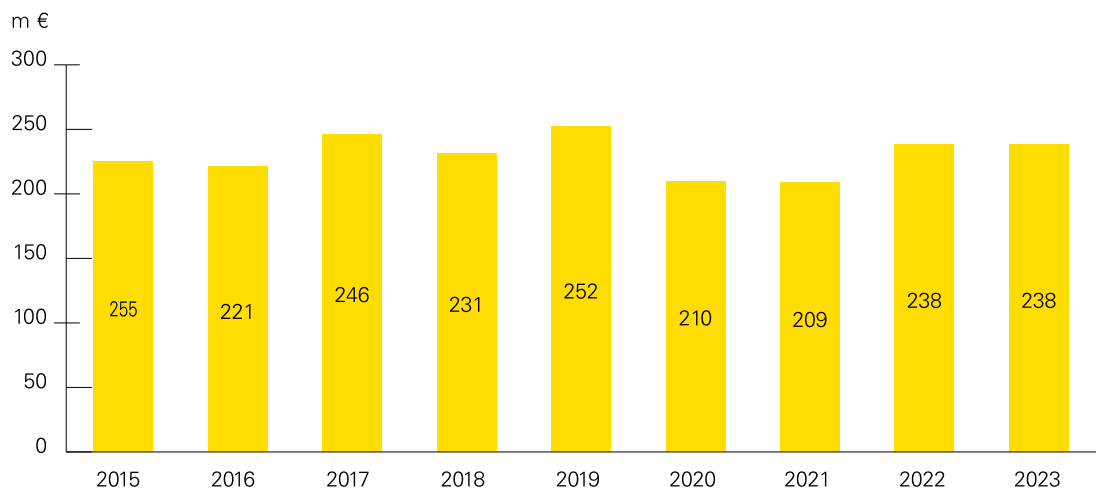
Expenditure on federal roads



Expenditure on state roads



Funding of municipal road projects under the Bavarian Municipal Transport Financing Act and the Bavarian Fiscal Equalisation Act





A 6/A 9, Nuremberg-East motorway junction flyover

MOTORWAYS

In collaboration with "Die Autobahn GmbH des Bundes"

Until the end of 2020, the Bavarian State Building Administration was responsible for the approximately 2,500 kilometres of motorways in Bavaria under contracts administered by the federal states. In the wake of the federal government's nationwide reform of motorway management, this responsibility was transferred to the newly established infrastructure company "Die Autobahn GmbH des Bundes" on 1 January 2021. "Die Autobahn GmbH des Bundes" is now responsible for the planning, construction, operation, maintenance and management of these major transport routes.

The successful structures of the Bavarian motorway authorities, with their offices and motorway maintenance depots, have been largely retained in the transformation to "Die Autobahn GmbH des Bundes". The North Bavarian branch of "Die Autobahn GmbH des Bundes", with offices in Bayreuth, Fürth and Würzburg, is responsible for the motorways in North Bavaria. In southern Bavaria, this role is performed by the southern Bavarian branch of "Die Autobahn GmbH des Bundes", with offices in Deggendorf, Kempten, Munich-Maisach and Regensburg. The headquarter of "Die Autobahn GmbH des Bundes" is located in Berlin.

Since the "Die Autobahn GmbH des Bundes" was set up, the funds needed to invest in the Bavarian motorways have been made available in sufficient amounts.

Over the next few years, the focus of investment at "Die Autobahn GmbH des Bundes" will be on the maintenance of existing structures based around the bridge modernisation programme.

In addition to improving road safety for road users and noise abatement, the focus of new construction, upgrading and widening measures is generally on increasing capacity.

In northern Bavaria, for example, the A3 motorway between the Biebelried and Fürth/Erlangen junctions is being widened to six lanes over a distance of around 76 kilometres, as is the A 6 motorway to the west of Feuchtwangen and between Triebendorf and Schwabach-West. In southern Bavaria, the eastern motorway ring road around Munich (A99) is being gradually widened to eight lanes. In addition, the extension of A 94 motorway, which will be the future link between Munich and Passau, is under construction between Kirchham and Pocking.

It is to be assumed that this will contribute to maintaining the good state of repair of the motorways in Bavaria.



A3, six-lane upgrade south of Regensburg with noise protection



A3, Unterrieden viaduct

OPERATIONAL SERVICE

Work performed by road maintenance depots

Our road maintenance service carries out a wide range of work to ensure that the Bavarian road network is kept in good and safe state of repair.

This is the job of our 64 road maintenance depots in all parts of Bavaria, with their around 2,200 orange-clad men and women - including district staff.



Hafenlohr depot of the Lohr road maintenance service

The operational service's work includes:

- Winter service
- Road maintenance - ongoing monitoring of the road network and repairs to keep the road in good condition
- Vegetation clearance and maintenance works as part of landscaping works
- Immediate measures and repairs to maintain traffic safety
- Maintenance of traffic signs and road equipment
- Repair of damage caused by accidents
- Pavement and shoulder cleaning

Winter maintenance and grounds maintenance account for almost half of the road maintenance service's activities.



Maintaining green spaces and keeping roads clear in Winter are essential services carried out by our road maintenance depots





LANDSCAPE PLANNING

Compensatory and replacement measures and flower strips

The protection of endangered species is the responsibility of society as a whole. Unlike much of our cultural landscape, green spaces along roads and pathways are not subject to the same pressures that come from use and recreation. These are the best conditions for promoting biodiversity. We can also enhance and connect plant and animal habitats on roadsides through

- specific maintenance concepts for high quality rough grazing land,
- phased mowing for wildflower strips to attract insects along some
- 33 kilometres of cycleways, with other meadows in the extensive area and with each other.

In the future, priority can be given to ecological aspects in the meadows in the extensive zone.

Areas with a high potential for enhancing biodiversity and strengthening the habitat network will be designated as 'priority areas' and managed accordingly, while the remaining areas should be maintained as 'standard areas'.

The road maintenance service will continue with the standardised maintenance of standard areas. The aim is to create fallow structures to enrich the habitat and structure by mowing sections every two years. Maintenance of the designated areas will be specifically set out in a maintenance and development plan, taking into account the initial state and development objectives.



Wildflower strips along our federal and state roads promote biodiversity

It is often unavoidable that construction projects have a negative impact on nature and the landscape. These impacts are compensated for fully, for example, by restoring water bodies, extensive grasslands and wetlands, lean and dry habitats, forests, shrubs or hedges.

However, the need for intervention can be minimised or avoided by taking preventive action, such as building green bridges, installing amphibian protection facilities, erecting habitat protection fences or through environmental construction monitoring.

So far, we have already created more than 3,000 hectares of compensatory and replacement areas. These are managed and maintained in accordance with the requirements of nature conservation and ecological considerations.



Compensatory measure for a road building project

ROAD SAFETY

Safe travel on our roads

We have high standards for designing and building roads that are safe for traffic and for eliminating safety deficits on existing roads. Extensive accident analyses provide important information on deficits, but also on the effectiveness of measures. For example, the use of intelligent traffic light systems or the conversion of accident-prone junctions to roundabouts have proven to be the most effective safety measures.

The expertise of local accident commissions is used to select the most appropriate improvement actions. The accident commissions received support from the local authorities and the Central Office for Traffic Safety.

In the accident commissions, the state building authorities work closely with the road traffic authorities and the police. Whether in cars, buses or lorries, on footpaths or on cycleways, or on bicycles or motorbikes, they always keep all road users in mind.

Modern infrastructure and road design are of particular importance in ensuring optimum road safety. As part of the new road safety programme "Bayern mobil - sicher ans Ziel", we will invest a total of EUR 500 million in construction measures by 2030. The aim is to address safety deficiencies, primarily on existing roads. The new road safety programme was developed with the Bavarian State Ministry of the Interior, Sport and Integration and was presented to the public in spring 2021.

The driving force behind our road safety programme is the Vision Zero initiative. Vision Zero is much more than a goal. It represents a paradigm shift leading to a comprehensive action plan. Vision Zero focuses on the vulnerability and error-proneness of human beings.

We want to protect the most vulnerable - pedestrians and cyclists, children and the elderly, and people with disabilities. Safety on country roads will also be further improved. Here, too, we want to address any safety shortcomings before people come to harm.



On the web at www.sichermobil.bayern.de
(in German only)





Underride protection for motorcyclists on winding roads

Safe junction design with roundabout





Mitigating traffic noise
with a noise barrier

NOISE PROTECTION

Quiet traffic

Noise abatement along roads is playing an increasingly important role in Bavaria. According to representative opinion polls, three quarters of the population feel disturbed by road noise, while a quarter of them feel very disturbed.

The number of people affected by road noise has increased as a result of ever-increasing traffic volumes, urban sprawl and the proximity of buildings to roads.

Providing the best possible protection for those affected under the given legal, economic and planning conditions is a particularly important task for the Bavarian State Building Administration.

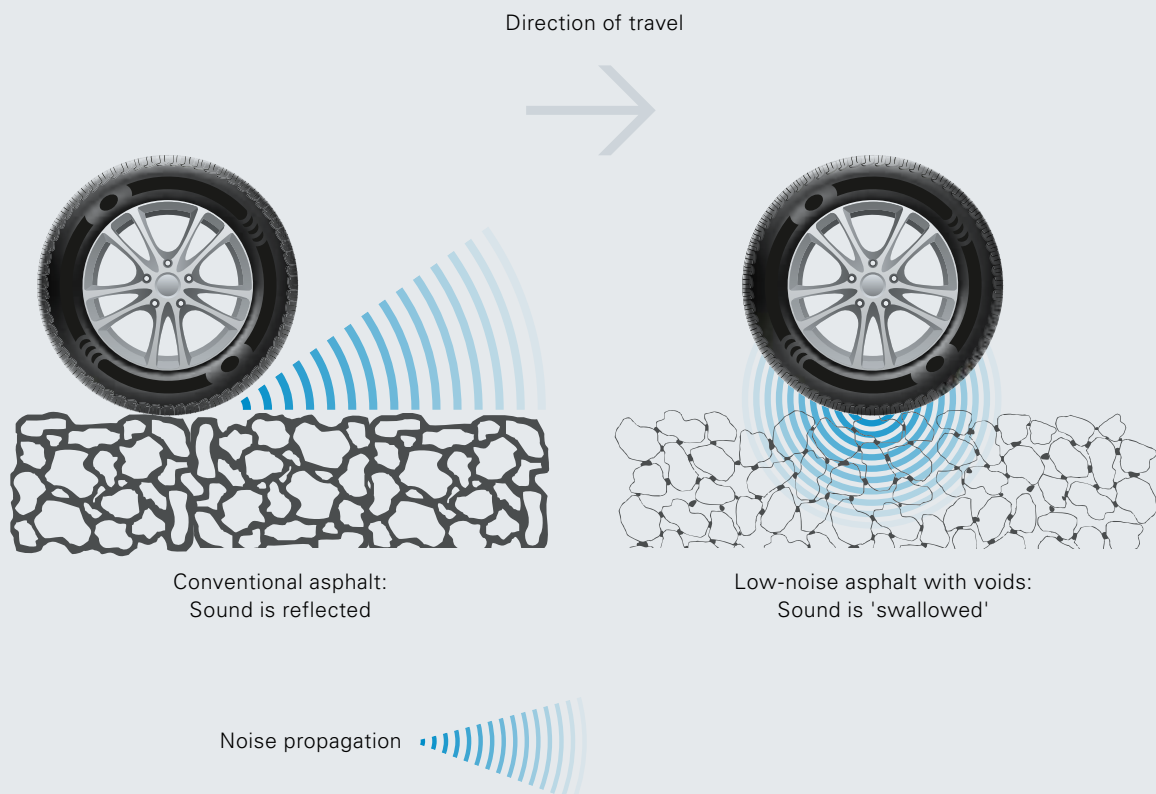
We are therefore constantly striving to ensure the best possible noise protection on roads. When roads are built or significantly altered, the statutory regulations on noise protection apply.

On existing roads, noise remediation measures are carried out voluntarily on the basis of budgetary rules.

The lowering of the trigger values for noise remediation in 2020 is a decisive element for more or better noise protection on existing roads. The 3 dB(A) reduction achieved is equivalent to halving the volume of traffic.

We are open to the idea of creating a more binding legal framework for noise remediation on existing roads, as is being discussed at federal level. The results remain to be seen.

How low-noise asphalt works

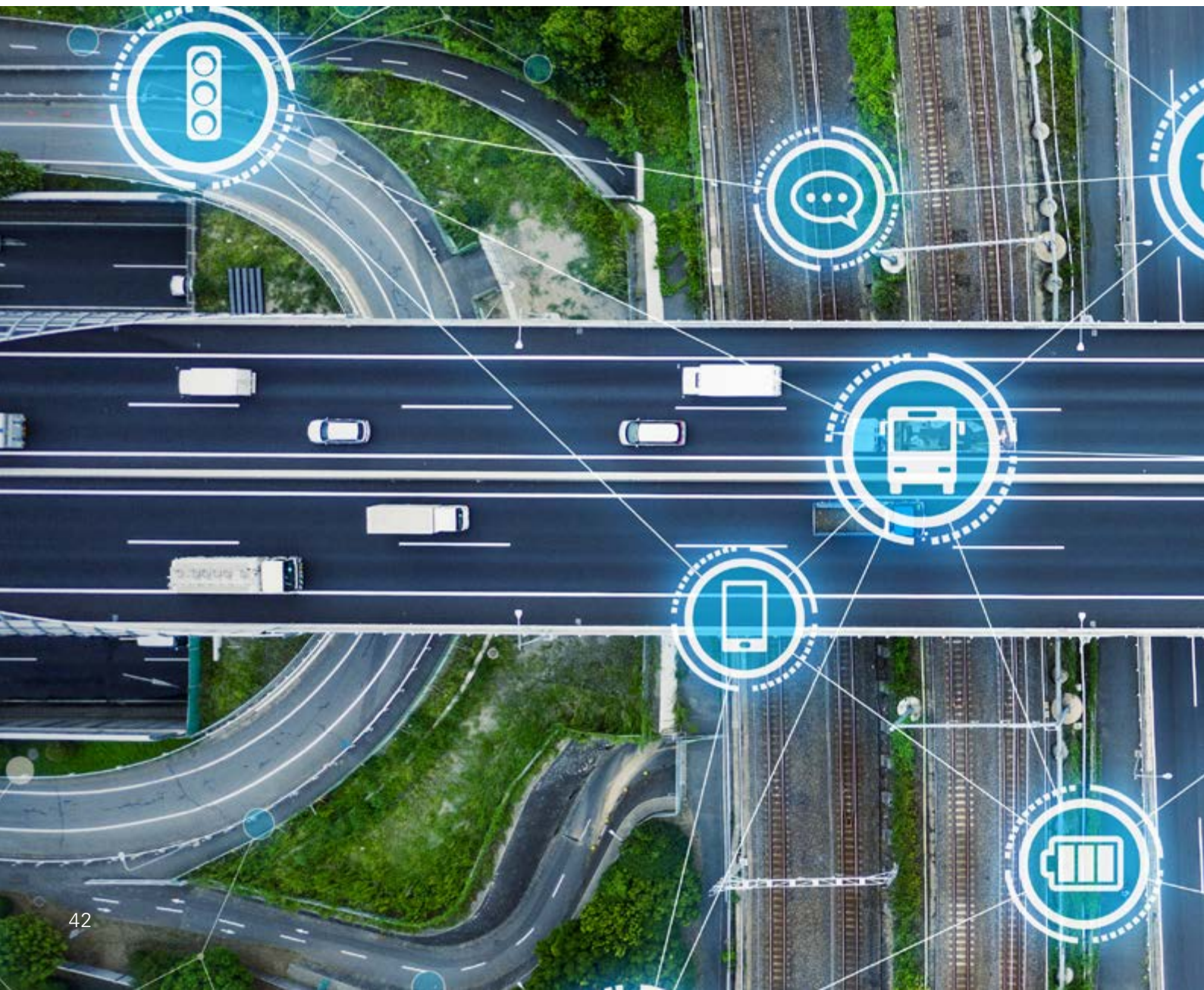


TRAFFIC MANAGEMENT

Intelligent traffic systems

We are making mobility more efficient, safer and more environmentally friendly with the gradual expansion of intelligent traffic systems on federal and state road network and the deployment of C2X technology for exchanging traffic-related information between vehicles and infrastructure.

- Coordination of road works throughout Bavaria to reduce congestion caused by road works
- Alerts about daytime and mobile roadworks using intelligent roadworks warning systems to increase road safety



- Provision of traffic information via a traffic information platform to improve the traffic flow
- Giving priority to emergency vehicles at traffic lights to ensure they can get through the junction safely and quickly
- Communication of traffic light switching times to vehicles to reduce unnecessary acceleration and braking
- Development of new technologies in collaboration with industry and trade



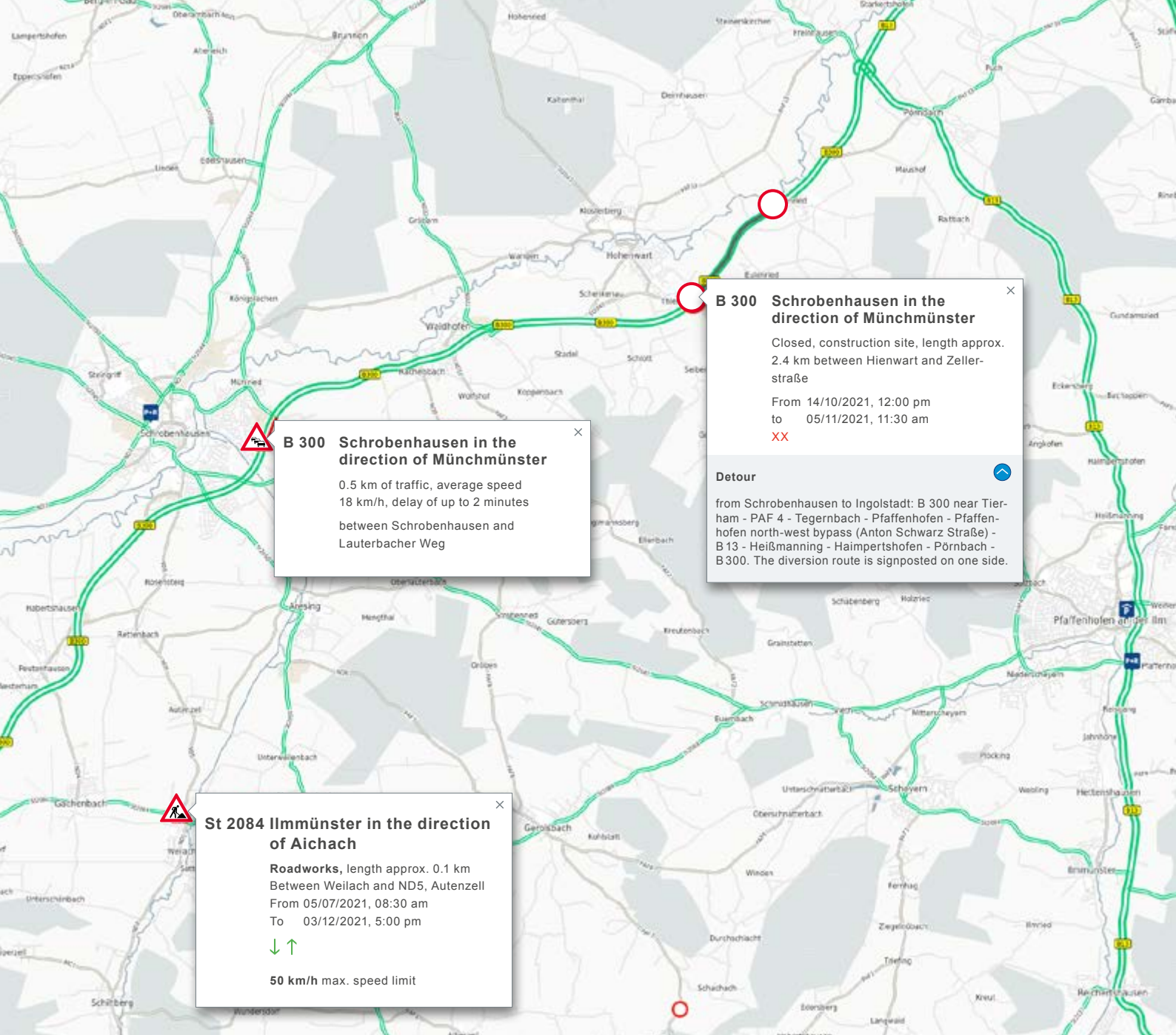
TRAFFIC INFORMATION

BayernInfo

Operated by the Bavarian State Building Administration, the BayernInfo system continuously collects and disseminates the most important traffic information from all over Bavaria. This is made available to citizens free of charge via the Internet portal "www.bayerninfo.de" and the "BayernInfo Maps" app.

- Real-time traffic data for 55,000 km of roads in Bavaria
- Latest information on roadworks, traffic jams and danger spots
- Live images from 320 webcams for key sections of roads
- Information on facilities at service stations and rest areas and on lorry parking occupancy
- Information on the e-charging infrastructure in Bavaria
- Route planner for all modes of transport





Map section from BayernInfo with display of ongoing roadworks and traffic information



Apple APP Store
<https://itunes.apple.com/de/app/bayerninfo-maps/id451116824?mt=8>



Google Play
<https://play.google.com/store/apps/details?id=com.ptvag.vib.bayerninfo&hl=en>





ROAD INFORMATION

BAYSIS

The Bavarian Road Information System (BAYSIS) is the central information platform of the Bavarian State Building Administration. BAYSIS maps the state's road network and its main features. It is used to record, maintain, analyse and provide information relevant to road use.

This information is available to the public on the Internet. The central application here is the BAYSIS map window as the interactive geographical information system. In addition, a wide range of information on the road network, including traffic data, upgrading programmes, etc., can be accessed online in tabular and cartographic form.



On the web at
www.baysis.bayern.de
(in German only)

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